



HCCA Significantly Increases Range and Reach of Training in 2012

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DIG IT!

THE NEWSLETTER OF THE
HEAVY CONSTRUCTION CONTRACTORS ASSOCIATION

Considering all of the means by which money flows into the HCCA, you might be surprised to learn that very little is unallocated in advance or serves to offset near-equal expenses. This means that only a small portion of the annual “revenue” of the Association is really available for “discretionary” use. Our training effort is where the lion’s share of this non-aligned income originates.

Training is important to the financial needs of the HCCA. We also offer classes that help our member firms acquire training needed for business requirements at a location in our working area.

Bringing together all the diverse pieces that need to be aligned for a training class to be pulled off successfully is no small endeavor. To date, our efforts have made a difference in the bottom line of the Association.

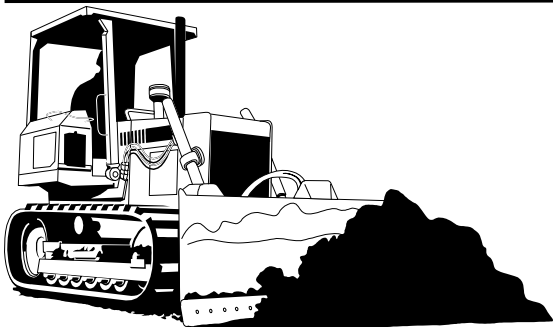
We, the HCCA, hope that in 2012 we will be even more successful. Our schedule for 2012 includes a number of new and rather unique subjects that are important to members and also respond to regulatory requirements..



A few years ago when VDOT first

promulgated the **Work Zone Traffic Control Safety** certification program, HCCA quickly rose to the challenge by offering classes in the Basic, Intermediate and Advanced programs. We began offering the Work Zone Basic in Spanish. In 2012, attendees of the 2008 classes will have credentials reaching the expiration date. The HCCA will continue to present the classes. Do not wait until the end of the expiration date to schedule classes as we anticipate a “rush” in fall 2012.

New to HCCA in 2012 will be **VDOT’s Erosion & Sediment Control Contractor Certification** program (known as ESCCC). These class have previously only been taught by the VTCA. There were only a few large classes held in Northern Virginia. This will be changing very early in 2012 as VDOT has now given HCCA (and HRUHCA) permission to offer the classes and issues the certificates. The HCCA will be able to conduct classes more frequently and with smaller enrollment. Classes will be limited to a maximum of 25



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NCCCO MOBILE CRANE OPERATOR TRAINING & TESTING

This 3 day class is for NCCCO Mobile Crane Operator training to prepare employees to take the NCCCO exams. The

class will cover basic safe lifting practices to include: Pre-use inspections: Proper crane setup and site conditions: Safe operating practices: Damage Prevention: Manufacturer's load charts



Introduction to MSHA and Virginia Mine Training

4 hour seminar on how to begin the development of a training plan required by MSHA & DMM.

MSHA & DMM—Annual Refresher Training

8 hours of required training for Annual MSHA & DMM Recertification

Introduction to MSHA and Virginia Mine Training

4 hour seminar on how to begin the development of a training plan required by MSHA & DMM.



EM385-1-1 US Army Corps of Engineers - Construction Safety Hazard Awareness Course for Contractors



OSHA-10 -- This course explores ten OSHA subparts that can be customized for specific needs. This is an indispensable course that all employees should have as a baseline in their continuing construction industry education.

OSHA-10 (Spanish) -- This course explores ten OSHA subparts that can be customized for specific needs. This is an indispensable course that all employees should have as a baseline in their continuing construction industry education.

OSHA-30 -- Necessary for any managers, foremen, superintendents, project managers and above. OSHA's thirty hour training program can be customized to cover 1926 standards. Foremost training module for all persons in supervisory and management positions.

OSHA-30 (Spanish) -- Necessary for any managers, foremen, superintendents, project managers and above. OSHA's thirty hour training program can be customized to cover 1926 standards. Foremost training module for all persons in supervisory and management positions.

OSHA-500 Trainer Course in Construction Industry Safety—This course is designed for personnel interested in developing safety and health programs in the construction industry. Special emphasis is placed upon those areas in construction that are the most hazardous, using OSHA standards as a guide. Attendees are required to have prior construction experience and need to take the OSHA 510 - Basic Course in Construction Industry prior to participating in this course. Course participants are briefed on effective instructional approaches and the use of visual aids and handouts. The OSHA 500 Trainer Course in Occupational Safety and Health Standards for the Construction Industry provides the latest adult training techniques and resources to the students who will become an instructor qualified to teach both the 10 and 30 hour OSHA voluntary compliance courses for the construction industry.

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OSHA-501 Trainer Course in General Industry Safety

This course is designed to present detailed information on how the provisions of the OSH Act may be implemented in the workplace. Rights and responsibilities under the OSH Act, the appeals process and recordkeeping are covered. OSHA's general industry standards and an overview of the requirements of the most frequently referenced standards will also be addressed. Upon successful completion of this course, graduates are authorized to conduct both a 10 and 30 – hour voluntary compliance course and are issued course completion cards from the Department of Labor.

OSHA-510 Basic Course in Construction Safety

This course is intended for private sector personnel covers OSHA policies, procedures, and standards, as well as construction safety and health principles. Topics include scope and application of the OSHA construction standards. Special emphasis is placed on those areas that are the most hazardous, using OSHA standards as a guide.

OSHA-511 Basic Course in General Industry Safety

This course is for private sector personnel covers OSHA policies, procedures, and standards, as well as general industry safety and health principles. Topics include scope and application of the OSHA general industry standards. Special emphasis is placed on those areas that are the most hazardous, using OSHA standards as a guide.

HCCA Announces Return of Driver Improvement Classes

If you have received a Virginia speeding ticket or other type of moving violation, our **DMV approved driver improvement clinic** is the perfect solution for your situation. This course can be taken for any of the following reasons: Virginia moving violation (speeding ticket, etc.), Court or judge ordered traffic school, DMV ordered driver improvement (too many points on license), Safe driver points on your Virginia driver license, Fleet training (ordered by employer or safety manager), Voluntary for personal use or insurance discount.

The Virginia DMV allows a driver to voluntarily complete a Virginia traffic school / driver improvement course in order to have 5 safe driving points (positive points) added to his or her driver license. These points are in addition to the safe driving points a driver receives each year for driving without committing any traffic violations. These points can be used to offset points from a Virginia speeding ticket or other traffic offense.

Drivers might be able to obtain a discount of up to 10% off their automobile insurance premium by voluntarily completing the course. In order to find out if you are eligible to receive this discount, please contact your automobile insurance provider.

Member firms should ask their insurance provider if there is a deduction on insurance if drivers take such a class.

HCCA, in conjunction with Better Driving Academy has scheduled many classes throughout the rest of 2012 at our Manassas facility. We encourage you to check the web site for the dates, costs, and registration details.

Political Corner

Northern Virginia Loses Its Clout in State Senate

The Washington Post

Northern Virginia has held considerable sway in the state Capitol for the past four years as Democrats from the region dominated the Senate. Six of the Senate's 11 committees, where chairmen decide the fate of legislation, were led by Northern Virginians. The majority leader, decider of which measures are actually voted on, was from Fairfax. All that has changed.

[Republicans last week seized control](#) of the evenly divided chamber and immediately stripped the heavily Democratic region of its sizeable clout. Now there's not a single Northern Virginian at the helm of a committee. The majority leader hails from Hampton Roads. And on top of that, two powerful, long-time senators from Northern Virginia retired.

Many in Northern Virginia's delegation fear they've lost the power to push through legislation to benefit the region and stop bills that hurt it.

Quite a comeuppance for a place that's home to nearly one-third of the commonwealth's 8 million residents, one known to refer dismissively to everything south of the Rappahannock River as RoVa, as in "rest of Virginia."

Northern Virginia has long complained that, with its fast-growing, affluent suburbs and vast business base, it foots the bill for more than its fair share of state services. Some of the region's leaders have for decades proposed — sometimes with tongue in cheek, sometimes not — seceding from the state. But now Northern Virginians feel like they've been shown the door.

"Northern Virginians have definitely lost influence," said Sen. Janet D. Howell (D-Fairfax), who lost her chairmanship of the Senate Privileges and Elections Committee and is waiting to hear whether she and two Northern Virginia colleagues will be reappointed as budget conferees. "We're the economic engine of the state, and our needs need to be addressed. And the danger is that they won't be."

Even some Republicans believe the region will be hurt. "I'd like to say it doesn't matter, but it does," said Del. David B. Albo (R-Fairfax), chairman of the House Courts of Justice Committee. "To the extent that Chuck Colgan" — the Democratic senator from Prince William who chaired the powerful Senate Finance Committee — "will not be there to kill things, it will be a loss."

In a state as large and diverse as Virginia, Northern Virginians are a troubled that senators from outside the region won't fully

appreciate their epic traffic woes and other pressing problems. This year, the region's legislators hope to fight Gov. Robert F. McDonnell's \$65 million cut to "cost to compete" funds, extra money the region has been allotted in the past to woo school employees in that high-dollar jobs market.

"These are things worked out in conference committee," said Del. Scott A. Surovell (D-Mount Vernon). "The cost to compete is something that only benefits Northern Virginia. It's something we need. Our teachers just cost a lot more." Northern Virginia senators also worry about their ability to block legislation on social issues that play very differently in the more racially diverse, better-educated and liberal Washington suburbs than in more rural parts of the state. "Last year, we had 24 bills coming over from the House of Delegates that were anti-immigrant, and we have no idea at this point if we can defeat them [this year]," Howell said.

As a committee chairwoman, Howell said, she did her greatest service to Northern Virginians by stopping what her constituents considered "bad legislation," such as proposals to expand gun rights or nullify federal legislation.

"When I'm in Northern Virginia, the issues I ran are very clear. And then I get on 95, and about the time I get to Kings Dominion, I go off into the twilight zone," Howell said. "Attitudes are different, issues are different, approach is different."

'Elect more Republicans'

Northern Virginia finds itself in this position for two reasons: The GOP is in charge and most of the region's senators — 10 of 13 — are Democrats. (Northern Virginia's three Republican senators are all too junior for chairmanships.)

[Lt. Gov. Bill Bolling](#) (R), whose tie-breaking votes allow his party to take control of a chamber that split 20-20 in [November's elections](#), offered a solution to Northern Virginians fretting about their diminished influence: "Elect more Republicans."

But Bolling, the state's jobs creation officer, also said the region has nothing to worry about. Senators from around the state will be attentive to the region's needs, he said, because the area is so vital to Virginia as a whole.

"Everybody here understands the importance of Northern Virginia," said Bolling, noting that 45 percent of the economic development deals hatched in the past two years have been in that part of the state. "Everybody jokes about the NoVa/RoVa thing, but I try to tell everybody around the state, 'If you live in RoVa, you'd better be glad there's a NoVa.'"

Sen. Ryan T. McDougle (R-Hanover), chairman of his caucus,

Political Corner

said the GOP takeover is “absolutely not” dire news for Northern Virginia, in part because the region’s business interests are in step with traditional GOP interests.

“It’s one of the most innovative technological areas of the commonwealth, and the country for that matter,” he said. “We’re going to put it in the best [business] climate to expand.”

Democrats: We’re united

Sen. Richard L. Saslaw (D-Fairfax), who was the Senate majority leader and is now minority leader, shares the view that the region will not suffer — but for very different reasons.

Northern Virginia will be able to flex some muscle during the budget process, Saslaw said, since Bolling has acknowledged that he is [not authorized to break tie votes on the budget](#); Republicans will need at least one Democratic vote for it to pass. And no Democrat will vote for the budget, Saslaw said, unless the region’s interests are protected — if only because Democrats statewide are upset about the takeover.

“Trust me, we’re unified,” he said. “There’s some pretty hard feelings there.” Those assurances fall flat for some Northern Virginians, who are also concerned that their influence will be further diminished by the retirements this year of two experienced Democratic senators: [Mary Margaret Whipple](#) of Arlington, who chaired the Rules Committee and her party’s caucus; and [Patricia S. Ticer](#) of Alexandria, who led the Senate Agriculture, Conservation and Natural Resources Committee. “It’s critical that our interests be represented,” said Sharon -Bulova (D), chairwoman of the Fairfax County Board of Supervisors.

Northern Virginian senators, who at least have seats on committees, aren’t the only ones feeling left out. Senator Phillip P. Puckett (D-Russell) complained on the Senate floor that Southwest Virginia did not have a single spot on the Senate Finance Committee.

“Even though you don’t have representation [on the committee], that does not preclude you in any way from introducing appropriate budget amendments,” Senate Majority Leader Thomas K. Norment (R-James City) replied. “And those budget amendments will be fairly and reasonably considered.”

That response irked some in the Northern Virginia delegation, both on behalf of Southwest and because of their own diminished power.

Sen. George L. Barker (D-Fairfax) said Norment’s comment boiled down to this: “‘We don’t need to have you be involved in decision-making. You submit what you want, and we’ll make the decision.’ . . . People need to be at the table and involved in the process rather than a supplicant going to the king.”

AGC Chief Economist Speaks to membership at the January Dinner Meeting



At the January Membership Dinner Meeting on Wednesday, January 18, 2012, the assembled members were treated to a special guest speaker who addressed them on the **2012 Economic Outlook for the Construction Industry**. Kenneth Simonson, the Chief Economist for the Associated General

Contractors of America spoke in some detail about conditions here in the Washington DC area for the construction industry in general, but with an emphasis on the impact upon transportation infrastructure.

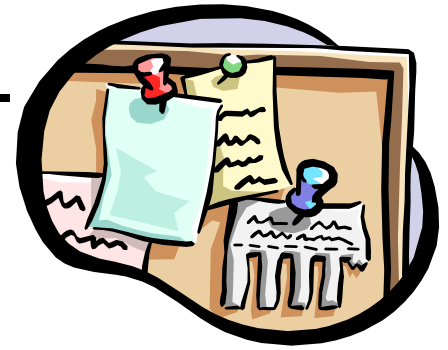
Mr. Simonson was quick to point out that while many economics and market-watchers predict a near-term healthy return of the economy, with the attendant improvements in the transportation infrastructure picture, he is less apt to jump to such conclusions based upon a critical, and long-term study of trends — direct and indirect — and their impact on the availability of dependable funding for the region’s transportation issues. He is of the opinion that the current malaise in the transportation construction industry may continue for at least two more years.

He also prefaced his remarks with concerns about the very unclear situation in Richmond and the coming battle between those that want to increase revenue for transportation infrastructure purposes and those that object to any increased taxes regardless of how they are couched.

His comments served to “confirm” the opinions and observations that we have been hearing anecdotally from local officials, talk-show hosts, pundits and others who watch the political interplay at the General Assembly.

Our advice—stay tuned!

Association Bulletin Board



HCCA extends its congratulations to **Clayton Jewell** on his selection as General Manager of Eastern Supply, effective January 1, 2012.

HCCA extends its congratulations to the following companies and individuals who were recognized at the **Fairfax County Land Conservation Awards** ceremony on January 20, 2012:

- **LARGE COMMERCIAL PROJECT**—The Anderson Company (Belvoir Business Center)
- **LARGE SINGLE FAMILY RESIDENTIAL**—American Infrastructure (Fosters Crest)
- **BEST PROTECTED ENVIRONMENTAL SENSITIVE SITE**—The Anderson Company
- **BEST TREE PRESERVATION**—SW Rodgers Company
- **OUTSTANDING CONTRACTOR**—The Anderson Company
- **OUTSTANDING SITE SUPERINTENDENT**—Denny Bungo, The Anderson Company

HCCA extends congratulations to **Daniel T. Cuccherini**, Safety Director for William A. Hazel Company on his appointment to the Virginia State Corporation Commission, Damage Prevention Advisory Committee as the Excavators Representative. Dan's term of office runs through December 2013.



Battle of First Manassas (Bull Run)

The HCCA has received a note of thanks from the “Discover Prince William and Manassas” for our sponsorship of the 150th Anniversary of the first battle of the Civil War. We have been presented with a magnificent aerial photograph of the reenactment, and this is available in the office and will be display at the February Dinner Meeting as well.



Dr. John Wells and Ms. Talibah R. Majeed representing the “Virginia Career Readiness Certificate Program” instructed the members present at the January Dinner Meeting about a joint venture between industry and the Virginia Community College system. By conducting a jobs analysis survey in order to determine the needs of a job description, and testing a job seeker to his or her present level of competency and educating the individual to raise their level of awareness, a marriage will occur between employer and job seeker. After an individual completes a standardized test he or she will earn one of three levels of Career Readiness Certificate based on their test performance;

Bronze – 30% comprehension; Silver – 65% comprehension; Gold – 85% comprehension. A more detailed explanation is available at 703-323-4068 from Ms. Majeed.

Gov. McDonnell Punts on Roads Plan

The Washington Post

IT APPEARS that Gov. Robert F. McDonnell (R) is throwing in the towel when it comes to finding a lasting fix for Virginia's most urgent problem: its crippling shortfall of transportation funding. Barring passage in the General Assembly of a major funding bill in the coming weeks — a tough sell with Mr. McDonnell's active support and a near-impossibility without it — the state's crumbling network of roads, rails and bridges is likely to keep deteriorating as congestion mounts.

The governor's inability, or unwillingness, to deliver major, ongoing new funding for transportation, despite the extravagant promises he made as a candidate, is a major failure, one that is likely to haunt Virginia for years.

The failure is compounded given that Mr. McDonnell, a Republican, is the first governor in more than a decade to face a General Assembly wholly controlled by his own party. Virginians may conclude that the GOP cannot resolve what the governor himself has identified as a full-blown crisis that threatens Virginia's long-term economic health.

Until a few days ago, Northern Virginia lawmakers and officials of Mr. McDonnell's administration hoped for a deal that would allow the state to stop digging itself deeper into a transportation funding hole. The deal might have taken the form of [indexing the gas tax](#), last raised in 1986, to inflation, or some other means of ensuring that Virginians pay for the transportation they use.

But a deal would have been possible only with a push by Mr. McDonnell. That push never came.

After canceling a news conference on transportation Wednesday, the governor quietly issued a press release Friday announcing, or rather whispering, the final pieces of his transportation package. The

latest measures, including [gimmicks such as selling naming rights for highways and bridges](#), will yield no significant cash.

The governor has repeatedly acknowledged the scale of the state's problem. He knows that Virginia will soon have no money to spend on new construction, since all available funding will be siphoned off just to maintain infrastructure.

Within existing restraints, Mr. McDonnell has taken some positive steps, like accelerating borrowing for projects already in the pipeline and proposing to funnel funds from surpluses or a booming economy — if they materialize — to transportation.

He has also offered legislation that would gradually shift revenue away from schools, public safety, and health programs to roads, rails, and bridges. But even if lawmakers enact that bill — a big if — it will produce just \$300 million annually by 2020. In the meantime, experts put the annual funding shortfall in the range of \$1 billion a year — now, not eight years from now.

Bills submitted by several Northern Virginia legislators, including Republicans, would raise more money, more quickly, by various means, including imposing a surcharge at the pump and indexing the gas tax. But Mr. McDonnell has been mum about those bills. Unfortunately, silence will not unclog Virginia's sclerotic roads.

At the heart of the problem are the anti-tax ideologues who dominate the Republican Party. The governor could have made an honest argument that indexing the gas tax — which would simply enable the state to keep pace with costs, and stop two decades of bleeding — is not a tax increase. In the end, he opted out of the fight and punted on the state's most pressing challenge.

Calendar of Events (Dates, Locations, & Times Subject To Change)

February

2/6-9 OSHA-500
2/9 Pool Hustlers Holiday
2/10 MSHA Refresher
2/10 Work Zone Basic (English & Spanish)
2/13 Rigger Level I
2/14-16 NCCCO Mobile Crane Class
2/15—Monthly Dinner Meeting
2/17 NCCCO Proctored Testing
2/21 Confined Space (Spanish)
2/22-23 Work Zone Intermediate
2/24 First Aid/CPR (English & Spanish)
2/25 Driver Improvement
2/28-3/2 OSHA-10/30 (English & Spanish)

March

3/1-2 OSHA-10 (English & Spanish)
3/5-8 OSHA-511
3/7 Fredericksburg Bowling Night
3/8 Rigger Level I (Spanish)
3/9 New Miner Training
3/9 Work Zone Basic
3/12 Incident Investigation
3/13 Job Safety Analysis
3/15-16 Work Zone Intermediate
3/16 MSHA Refresher
3/19-23 EM385-1-1
3/21 Monthly Dinner Meeting
3/23 MSHA Refresher
3/24 Driver Improvement
3/26-29 OSHA-501
3/30 First Aid/CPR
3/31 PAC Pigeon Shoot

April

4/3-4 OSHA-10
4/10-11 OSHA-30
4/12-13 Work Zone Advanced
4/14 Work Zone Basic
4/16-18 First Aid/CPR Instructor
4/18 Monthly Dinner Meeting
4/20-21 Work Zone Intermediate
4/21 HAZWOPER-8
4/22 Driver Improvement
4/23-27 MAZWOPER-40
4/27 Dinner Cruise
4/30-5/3 OSHA-510



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